

JAN HWA-MTG  
10/25

## Chandler Windom

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**From:** Kathy Clay  
**Sent:** Wednesday, October 27, 2021 4:27 PM  
**To:** Chandler Windom  
**Cc:** Butch Gosselin  
**Subject:** RE: Request for PRC Legacy Lodge

Hi Chandler:

Legacy Lodge was not reviewed or inspected under our jurisdiction as it was owned by the State. For the building to reopen, the following must be met:

- All life safety systems shall be inspected
- Building fire alarm system must be monitored by an alarm company
- Fire inspection shall be conducted to ensure other life safety features are in place; emergency egress lighting, elevator operation, etc.
- Electrical Inspection shall be conducted as well.

We realize this structure is several years old and will be inspected to the year of the code it was built under, however, the rules of IFC Chapter 11 will apply and will be followed.

Kathy Clay  
Battalion Chief Fire Marshal  
Jackson Hole Fire/EMS  
(desk) 307-732-8506  
Facebook JHFireEMS  
[www.jhfire-ems.org](http://www.jhfire-ems.org)



"Getting Ahead of the Call"





2075

# STAFF MEMO

November 12, 2021

**TO: Chandler Windom, AICP - Teton County Senior Planner**

Delivery via email: [cwindom@tetoncountywy.gov](mailto:cwindom@tetoncountywy.gov)

**FROM: Amy Ramage, PE - Teton County Engineer**

**RE: CUP2021-0005 & PUD2021-0001 Legacy Lodge - Rafter J**

Chandler,

Thank you for the opportunity to comment on the above referenced application for a conditional use permit (CUP) and PUD to accommodate proposed future housing development in the existing Legacy Lodge facility. I offer the following comments from my perspective within Public Works:

## Parking

According to the application, this development will provide 57 units of employee housing and an associated management office. There are 36 existing parking spaces. Applying standard formulas of 2.5 spaces per unit would require 142 parking spaces. While I am an advocate for reducing standard parking formulas to reduce the infrastructure burden for affordable housing units and encourage less single occupant vehicle use, it seems that the number currently provided is substantially inadequate to meet the needs of residents and employees, even with methods encouraging residents to not have a car, such as bike lockers and robust transit service. This location is also somewhat remote from other supportive infrastructure like grocery stores and schools and further limits residents' ability to live car-free compared to locations that are within the Town of Jackson.

The concern with having a great deficit of parking is that the adjacent roadway, Big Trail Drive, will inevitably bear the burden of overflow parking, even if it is prohibited. The adjacent roadway is not designed to accommodate parking and puts the burden on the Rafter J ISD/HOA to enforce the issues that come with rogue parking and fix roadway shoulders that will become denuded and need signage. Perhaps there is a route to providing parking along the road if Rafter J ISD/HOA was a willing partner, however there are safety issues that would need to be addressed.

In addition, the lack of available parking spots can inadvertently "screen out" people who are critical workers who would greatly benefit from this housing opportunity simply because there is no place for them to park their vehicle that they need for work.

## Transportation Demand Management (TDM) Plan

The new Legacy Lodge should have a well-defined TDM plan as part of their approval to put in place measures to reduce single occupant vehicle trips generated by this land use. The TDM plan should

identify multi-modal opportunities that could be supported by infrastructure such as proper bike storage and parking and e-bike charge stations. Programmatic solutions such as ride share, shuttles or rewards could also be considered. It is recognized that while limiting or pricing/leasing parking can be part of a TDM plan, it seems the amount available is well below the threshold that is reasonable to expect, especially in an area that is outside of the corporate limits of the Town and lacks nearby supportive services.

Refuse

As part of this conditional use permitting, trash and recycling facilities should be retrofitted to be bear-proof if they are not already.

Thank you for the opportunity to review this proposal.

**Chandler Windom**

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**From:** Billy Nunn  
**Sent:** Friday, November 12, 2021 2:37 PM  
**To:** Chandler Windom  
**Subject:** RE: Request for PRC Legacy Lodge

No comments from me at this time.

*Billy Nunn*  
Building Official  
Teton County WY  
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[bnunn@tetoncountywy.gov](mailto:bnunn@tetoncountywy.gov)  
salutem aedificationem





To: Chandler Windom  
*Senior Planner, Teton County Planning and Building*

From: Stacy Stoker  
*Housing Manager, Teton County Housing Department*

Re: CUP2021-0005 & PUD2021-0001  
 Legacy Lodge

Date: November 16, 2021

The applicant is requesting to convert an existing assisted living Institutional Use located at 3000 W Big Trail Drive in Rafter J to residential use and is requesting a Conditional Use Permit and an Amendment to the Rafter J PUD.

The applicant proposes converting the existing 57 units to "Workforce Housing" and also uses the term "Employee Housing" in the application. They have not provided a Housing Mitigation Plan so it is not clear what the Housing Requirement generated by this change of use may be. If the change of use generates a Housing Requirement to provide Units under LDR 6.3, the units would require an Affordable Deed Restriction specific to the Income Range of the required unit(s).

The applicant is proposing not to restrict any of the units but are calling the units "Workforce Housing". The applicant has asserted that this change of use is providing "Workforce Housing", which is a benefit to the community. The Housing Department agrees that these units can be a benefit to the community but will only be a guaranteed benefit to the community if they are restricted. Workforce Housing as defined in the Housing Department Rules and Regulations requires Deed Restrictions. This is the only method that ensures the units will be used for housing the Workforce *in perpetuity*.

The Housing Department requests that a Housing Mitigation Plan be submitted prior to approval in accordance with the LDRs and the Housing Department Rules and Regulations.

All restricted units are required to comply with the Livability Standards in the Jackson/Teton County Housing Department Rules and Regulations.

The following shall occur prior to issuance of any Permits.

1. The Livability Standards Questionnaire shall be completed and submitted to the Housing Department for review along with floor plans that include dimensions and a functional furniture placement diagram.

2. A letter from the Housing Department will be issued to the applicant stating whether the unit(s) are approved or whether there are required changes.

3. A Livability Standards Approval Letter is required to be submitted to the Planning Department along with submittal for Building Permit.

The following shall occur prior to issuance of Certificate of Occupancy:

1. The applicant or applicant's agent(s) shall attend a Compliance Conference with the Housing Department.

2. The Housing Department shall inspect the unit.

3. A restriction drafted by the Housing Department using the applicable approved Restriction Template will be recorded on the units/property. The applicant will be responsible for payment of recording fees.

Thank you for the opportunity to review this application. Please contact me with any questions.





# PUBLIC WORKS - PATHWAYS

**Brian Schilling**, *Pathways Coordinator*  
bschilling@tetoncountywy.gov  
307.732.8573

November 20, 2020

**TO: Chandler Windom, Senior Planner / Teton County Planning Department**

**RE: CUP2021-0005 and PUD2021-0001 - Stage Stop/Legacy Lodge, Lot 333 Rafter J**

Dear Chandler:

Thank you for the opportunity to review the PUD and CUP submitted by Stage Stop, Inc. for residential housing at Legacy Lodge in Rafter J. The Teton County Pathways department submits the comments below as considerations for this application.

## **Pathway crossing / Big Trails Dr. access driveway**

The pathway crossing at the Legacy Lodge access driveway from Big Trails Dr. has several existing issues that will need to be remedied to ensure safe conditions for pathway users and motorists given the expected significant increase in daily trips to and from the development.

The existing concrete apron has a drainage issue that causes water to pond within the limits of the pathway. This creates very hazardous conditions, especially during winter months when the pathway can be completely obstructed by ice and/or standing water and slush.

The design of the pathway crossing and access driveway does not reflect the current best practices for pathway crossings at minor side streets and access drives. The FHWA Guide for Small Town and Rural Multimodal Networks describes strategies for designing intersections of sidepaths and driveways that enhance safety by establishing clear right-of-way assignments, slowing speeds, and maintaining visibility for all users. (See the attached excerpt from the FHWA guide for details).

In order to remedy both the drainage and the safety issues, the driveway access will need to be reconfigured to provide an elevated crossing with appropriate striping and signage. The pathway may also need to be realigned slightly in order to provide sufficient offset from the adjacent roadway.

Pathways staff will be happy to provide design assistance and guidance for this.

## **Transportation Demand Management**

The applicant should evaluate and implement transportation demand management strategies to reduce the total number of trips to and from the proposed development. The Teton County Comprehensive Plan and Integrated Transportation Plan both note transportation demand management programs as a

primary trip reduction tool for developments that potentially generate a large number of trips. Some ideas the applicant could explore:

- Transit service to Rafter J (staff suggests that the applicant consult with START on future plans for service to the South Park area)
- Providing secure and convenient bicycle parking/storage facilities for residents
- Carpooling or shuttles for residents and on-site staff
- E-bikes and e-bike charging stations for residents and staff
- Integration of transit and active modes (biking and walking) to facilitate use of transit

### **Bicycle Parking**

Per the above discussion on TDM strategies, one of the easiest ways to encourage active modes and reduce vehicle trips is to provide convenient, secure bike parking for residents and visitors/staff. Pathways staff will be happy to assist with specific on-site design recommendations and layouts. The following comments are provided for general, preliminary guidance:

- Given the location and type of use of the proposed development, staff expects that there will be a high demand for bicycle parking.
- Staff supports crediting the bike parking toward the development's overall parking requirement.
- The bike parking should ideally be a mix of short-term (for visitors, frequent/regular use) and long-term (for employees, residents, infrequent use/storage). For this development, we anticipate the need for long-term parking will be significantly greater than for short-term.
  - Short-term parking (for visitors or guests parking for a few hours or less): the recommended style for short-term bike parking is one or more "single inverted-U" racks. **"Wave," "ribbon," and "toaster" style racks shall not be used.** The best location for a rack area is immediately adjacent to the entrance it serves. The rack area should be as close as or closer to the front entrance than the nearest car parking space, visible from the front entrance, hardscaped, and should not obstruct pedestrian flow. Short-term parking supply will be a factor of the number of staff and the expected number of visitors to the site, which may not be currently not known.
  - Long-term parking (for employees/residents parking for more than a few hours—i.e. all-day or overnight): the recommendations for long-term parking include providing a secure, conveniently-accessed, well-lit, covered area with racks or lockers that will protect bikes from rain, snow and other elements and deter bike theft. The area does not have to be immediately adjacent to the access door for the residence, but should be located in a secure or monitored location or in a locked enclosure. Clustered inverted-U bike racks, wall racks, and external bike lockers (i.e. not an indoor closet) are all appropriate options.
- At least one bike parking/storage space should be provided for each unit.
- U-rack bike parking should be constructed on a concrete pad. Grass or natural surfaces will quickly deteriorate into mud from foot traffic during wet seasons and will be difficult to keep clear of snow. Also, concrete provides a more secure mounting surface for the racks and will discourage theft.



- Staff supports including the bike parking towards the landscape surfacing requirement so that bike parking does not detract from the applicant's landscape requirements. Conversion of one or more car parking spots to bike parking would also be supported.
- Rack details and locations should be shown on site plans.
- Jackson Hole Community Pathways will be happy to provide additional background information and guidance on site selection, layout, rack specification, and rack installation.

Thank you again for the chance to provide comments on this application. I look forward to working with you and the applicant on addressing these items.

Brian Schilling, Pathways Coordinator

Cc: Amy Ramage, Teton County Engineer  
Heather Overholser, Teton County Public Works Director





DECEMBER 2016

# Small Town *and* Rural Multimodal Networks



U.S. Department of Transportation  
**Federal Highway Administration**





# Sidepath

## INTERSECTIONS

Operational and safety concerns exist where sidepaths cross driveways and intersections. Refer to section 5.2.2 of the *AASHTO Bike Guide 2012* for an identification of potential design issues. Design crossings to promote awareness of conflict points, and facilitate proper yielding of motorists to bicyclists and pedestrians.

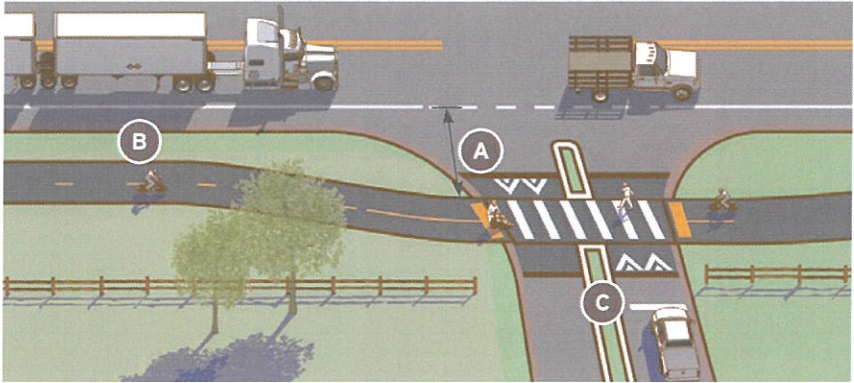
### DESIGN STRATEGIES

Collision risk increases as the speed and volume of the parallel roadway increase. The *AASHTO Bike Guide 2012* lists a variety of design strategies for enhancing sidepath crossings including:

- Reduce the frequency of driveways.
- Design intersections to reduce driver speeds and heighten awareness of path users.
- Encourage low speeds on pathway approaches.
- Maintain visibility for all users.
- Provide clear assignment of right-of-way with signs and markings and elevation change.

### DESIGN DETAILS

- A** Maintain physical separation of the sidepath through the crossing. Sidepath separation distance may vary from 5 ft–24 ft (1.5–7.0 m). Refer to *Table 4-2*.
- Use small roadway corner radii to enforce slow turning speeds of 20 mi/h or less. On a high-speed roadway, a deceleration lane may be necessary to achieve desired slow turning speeds.



**Figure 4-11.** Separation distance should be selected in response to speed and traffic intensity. The pathway may need a shift in horizontal alignment in advance of the crossing to achieve desired separation distance. As speeds on the parallel roadway increase, so does the preference for wider separation distance.

**Table 4-2.** Sidepath Separation Distance at Road Crossings<sup>(vi)</sup>

Adjacent Road Speed Limit (Mi/h)	Recommended Sidepath Separation Distance at Crossings
< 25 mi/h	6.5 ft (2.0 m)
35–45 mi/h	6.5–16.5 ft (2.0–5.0 m)
≥ 55 mi/h	16.5–24 ft (5.0–7.0 m)

\*Separation distance may vary in response to available right of way, visibility constraints and the provision of a right turn deceleration lane.

- B** The roadway and path approaches to an intersection should always provide enough stopping sight distance to obey the established traffic control, and execute a stop before entering the intersection (*AASHTO Bike Guide 2012*).
- C** Where possible, include raised median island on the cross street to provide additional safety and speed management benefits.
- Configure crossings with raised speed table or “dustpan” style driveway geometry to create vertical deflection of turning vehicles. This physically indicates priority of path travel over turning or crossing traffic and helps reduce the risk associated with bidirectional sidepath use.<sup>(vi)</sup>
  - Use crosswalk markings to indicate the through crossing along the pathway. Continental crosswalk markings are preferred for increased visibility. At low-volume residential driveways, crosswalk markings may be omitted.<sup>(vi)</sup>
  - Use stop or yield line markings in advance of the crossing to discourage encroachment into the crosswalk area.



# Sidepath

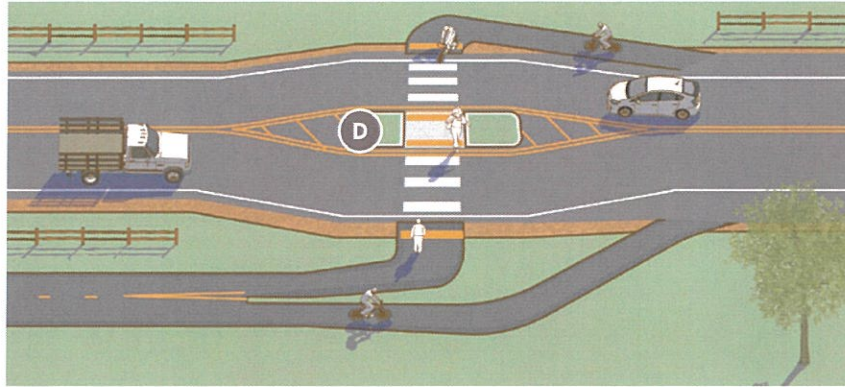


Figure 4-12. Transition from a sidepath on one side to shoulders on each side of the road.

## ACCESSIBILITY

A sidepath is intended for use by pedestrians and must meet accessibility guidelines for walkways and curb transitions. Sidepaths are required to be accessible by all users, including those with mobility devices and visually-impaired pedestrians.

## IMPLEMENTATION

Where sufficient roadway width or right of way is available, designers should consider the simultaneous provision of both sidepaths and bicycle accessible shoulders to serve a diverse range of user types.

### Minor Street Crossings

Give sidepaths the same priority as the parallel roadway at all crossings. Attempts to require path users to yield or stop at each cross-street or driveway promote noncompliance and confusion, and are not effective. Geometric design in these cases should promote a high degree of yielding to path users through geometric design.

- Landscaping, barriers, or other visual obstructions should be low to provide unobstructed sight of the crossing from the major street. Both motorists and path users should have a clear and unobstructed view of each other at intersections and driveways.
- Consider using a R10-15 RIGHT TURN YIELD TO PEDESTRIANS at street crossings with right turn interactions.

### Connections with On-Street Bikeways

Where a sidepath terminates, it may be necessary for path users to transition to a facility on the opposite side of the road.

- D** Designs should consider the desire for natural directional flows, and the potential for conflicts with adjacent traffic. Use median islands and horizontal deflection of the roadway travel lanes to slow motor vehicle traffic and offer improved crossing conditions for path users.