

**RAFTER J IMPROVEMENT AND SERVICE DISTRICT**  
**2951 Big Trail Drive, Jackson, WY 83001**  
**Phone: 307-733-5262, Fax: 866-748-1908**  
**Email: RafterJ@onewest.net website: www.RafterJ.org**

**Street and Driveway Repair Specifications**

Road and Driveway Repairs – Each Lot owner is responsible for street and driveway repairs required from replacing or repairing their private water and sewer service lines. Private water service lines begin at the curb stop (valve near the street) and extend to their house. Private sewer service lines begin at the sewer main connection in the road or utility easement and extend to their house.

**Construction**

All trenches shall be compacted to prevent settlement. Saturated soil materials may require replacement with suitable gravel backfill to provide a stable foundation for the streets and driveways.

Patching - Street patches shall be cut 12” wider than the trench. Street patches shall be saw-cut to provide a straight, vertical patch edge. Install pit run gravel subbase, crushed gravel base, and asphalt pavement in conformance to the layer depths specified on the Street Patching Detail. All existing pavement surfaces (including patch edges) that are to be covered with new asphalt pavement shall receive a tack coat prior to installing the asphalt mix.

Pit Run Gravel Subbase – Place and compact gravel subbase material to provide a non-yielding stratum, verified by proof rolling with construction equipment.

Crushed Gravel Base – Place and compact gravel base material, utilizing moisture conditioning to obtain at least 95% density, within plus 2% to minus 4% of optimum moisture as determined by AASHTO T-180.

Asphalt Pavement - Place and compact to provide a smooth, free-draining surface. Finish asphalt pavement layers exceeding 3” in depth shall be placed in two lifts. Saw cut and tack joints abutting existing pavement surfaces (including pavement recently installed the day before). Surface depressions shall not exceed ¼” depth when measured using a 10-ft long straight edge. The final compacted density of the asphalt pavement shall range from 92% to 96% of the maximum theoretical density of the asphalt mix.

Overlays - All existing pavement surfaces to be overlaid shall receive a tack coat prior to installing the asphalt mix. The District intends to install 1.5” asphalt overlay on top of the existing street surface. The overlay work will include construction of 1’ to 2’ wide transitions from the road edge to the existing driveway pavement. This work includes saw cutting the existing asphalt and concrete driveways to match these transitions. All driveway repair and re-surfacing work must be completed to accommodate a street surface 1.5” higher than the presently existing asphalt pavement. See Driveway Approach Detail.

Street and driveway patching and surfacing and associated work shall conform to the Wyoming Public Works Standard Specifications, 2001 Edition. (Contact the Wyoming Contractor’s Association at 307-632-0573 to obtain a copy.)

Street patching areas must be observed by the District prior to installing asphalt pavement. The District may also require compaction tests on the trench backfill, gravel base, and asphalt pavement. You are responsible for contacting the District (Chuck McCleary at 307-733-5262) to schedule the inspections.

### **Street Surfacing Material Specifications**

Pit Run Gravel Subbase – Screened or unscreened gravel obtained from natural river alluvial deposits with stone generally not exceeding 6” in size. Pit run gravel shall be excavated and handled to provide a well graded material.

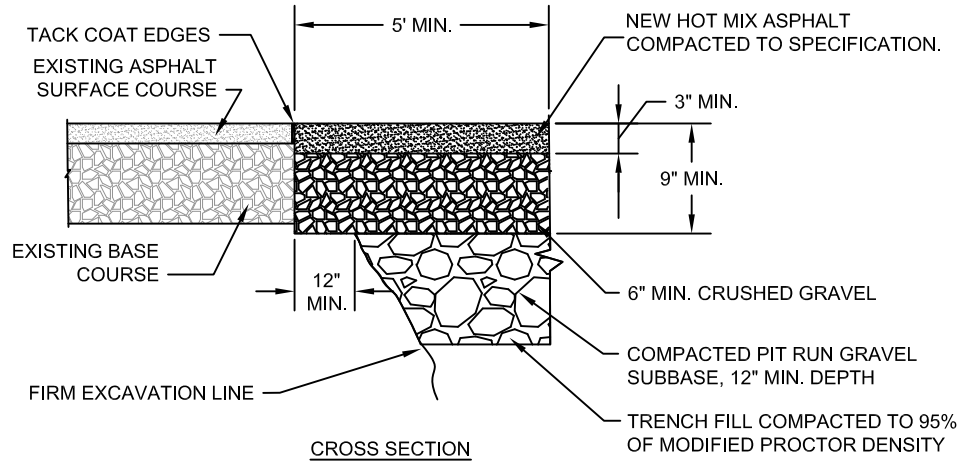
Crushed Gravel Base – Grading H, 100% material passing a 1” sieve.

Asphalt Tack – Emulsified Asphalt SS-1, applied at 0.25 lbs per square yard.

Asphalt Pavement – Commercial mix, Type III Aggregate, WYDOT Grading C, PG 58-28 Asphalt Cement. The asphalt mix design shall be prepared by a qualified, independent laboratory, utilizing current design and testing standards.

### **Attachments**

- 1) Street Patching Detail
- 2) Driveway Approach Detail




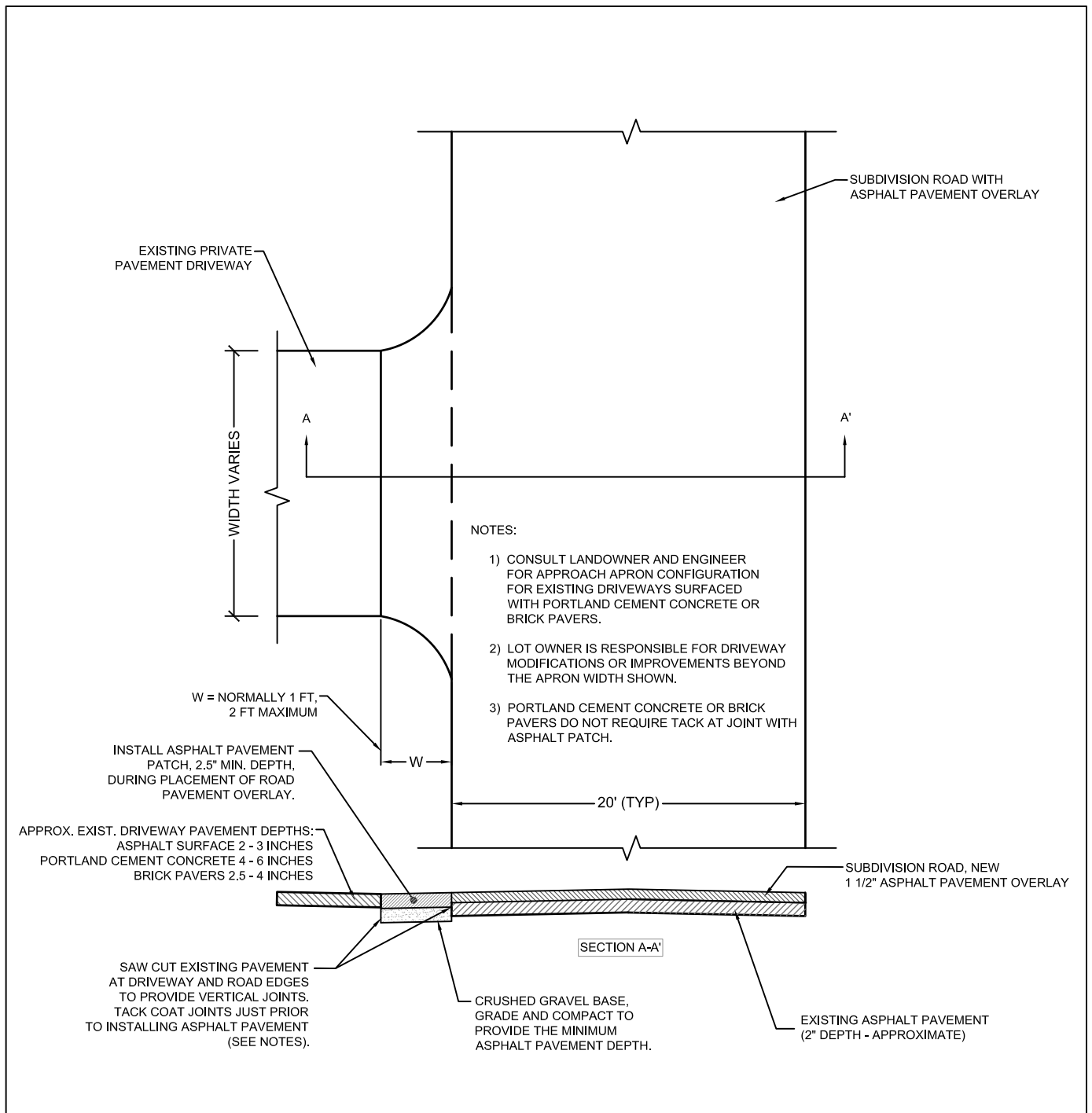
ASPHALT PATCH DETAIL

NOTES:

1. REPLACEMENT ASPHALT THICKNESS SHALL BE 1" GREATER THAN EXISTING AND NO LESS THAN 3" MINIMUM.
2. ASPHALT SHALL BE PLACED IN TWO (2) LIFTS, EACH NO LESS THAN 1- $\frac{1}{2}$ " IN THICKNESS, AND COMPACTED TO 92-96% OF MAXIMUM DENSITY.

**STREET PATCHING DETAIL**  
NTS

DRAWING TITLE: <b>STREET PATCHING DETAIL</b>	 <p style="margin: 0;"><b>RENDEZVOUS ENGINEERING, P.C.</b></p> <p style="margin: 0;">P.O. BOX 4858 JACKSON, WYOMING 83001 25 SOUTH GROS VENTRE STREET PHONE - 307.733.5252 FAX - 307.733.2334</p>	DRAWING NUMBER: <b>15.00</b>
PROJECT TITLE: <b>RAFTER J INFRASTRUCTURE</b>		PROJECT NUMBER: <b>04-060</b>
		Prepared By: TPF
		Date: 04/23/09
		Rev. Date:



NOTES:

- 1) CONSULT LANDOWNER AND ENGINEER FOR APPROACH APRON CONFIGURATION FOR EXISTING DRIVEWAYS SURFACED WITH PORTLAND CEMENT CONCRETE OR BRICK PAVERS.
- 2) LOT OWNER IS RESPONSIBLE FOR DRIVEWAY MODIFICATIONS OR IMPROVEMENTS BEYOND THE APRON WIDTH SHOWN.
- 3) PORTLAND CEMENT CONCRETE OR BRICK PAVERS DO NOT REQUIRE TACK AT JOINT WITH ASPHALT PATCH.

**DRIVEWAY APPROACH DETAIL**

NTS

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DRAWING NUMBER: <b>15.10</b>
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Prepared By: TPF
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Rev. Date: 05/21/09